



## Corridors

Berkley's high traffic roadways, or corridors – Twelve Mile, Coolidge Highway, Eleven Mile, Woodward and Greenfield – provide some of the most memorable places in the City. Residents on their daily commute, by foot, car, bicycle or bus, likely travel one or more of these corridors. They host Berkley's popular community events – such as the Art Bash, the Woodward Dream Cruise and more. They are also where Berkley residents and visitors eat, shop, worship, play and live.

Respondents to the 2020 Survey indicate a strong desire for commercial, entertainment, and mixed uses on Berkley corridors but often physical constraints, from street layouts to parcel depths, have constrained redevelopment of these corridors. This chapter lays out steps for corridors overall and in particular how they can become lively, vibrant places.

### Principles

Principles from the Future Land Use Chapter that apply to corridors are:

- Enhance corridors as vibrant business areas.
- Blend new housing types using good design to complement existing character.

### Previous Plans

Previously adopted plans are relevant to corridors include:

- 2012 Woodward Transit-Oriented Development Corridor Study for South Oakland County
- 2012 Multi Modal Transportation Plan
- 2018 Downtown Design Guidelines
- 2019 Downtown Plan
- 2020 Parks and Recreation Plan Update

### Land Uses

The following future land use categories are appropriate in corridors:

- Downtown
- Gateway Corridor
- Industrial/Retail
- Residential Corridor

## CORRIDORS

The Master Plan envisions vibrant and inviting corridors, each with their own sense of place. The following strategies should be used in all corridors throughout the City:

### Offer Transportation Choices

Corridors should be improved with safe transportation choices for pedestrians, cyclists and transit riders as well as motorists and vehicle passengers. Possible improvements include bike lanes, integrating bus stops with new development or property redevelopment, and expanded transit service. The creation of “transit nodes” on corridors is an economic development tool to increase foot traffic and economic activity at key intersections. A transit node has wider sidewalks and more intensive development (i.e., taller buildings built to edge of the sidewalk) at transit stops. Nodes should be implemented on Twelve Mile Road, Coolidge Highway, and Woodward Avenue.

### Encourage Corridors to become Complete Streets

Complete Streets encourages safe street design accessible to all users regardless of their age, ability, or transportation choices. In 2018, the City adopted a Complete Streets Ordinance making the policy of the city to encourage complete streets through planning and zoning approvals. In addition, the City, through public-private partnerships, should provide targeted right-of-way (ROW) improvements to ensure greater connectivity and easier navigation for motorists, transit users, bicyclists, and pedestrians. Complete street principles should be explored on Twelve Mile Road, Woodward Avenue, Eleven Mile Road, Greenfield Road, and continued along Coolidge Highway. Any roadway configuration changes or improvements on corridors that border other municipalities – Eleven Mile, Greenfield and Woodward – should be in cooperation with adjacent communities and the agency that controls the street.

### Improve the Pedestrian Experience

The pedestrian experience along corridors can be improved through design and investment where people walk. This includes the area between the back of curb and front buildings facades, known as the pedestrian realm. A well designed pedestrian realm includes sidewalks and crosswalks of the appropriate



The MoGo Bike station off of Twelve Mile is an example of transportation choices beyond the automobile in Berkley.

Source: CWA



Coolidge during the road diet as a Complete Street with separate areas for vehicles, bicycles and pedestrians.

Source: City of Berkley



The bench, flowers, trash receptacle and lighting on Twelve Mile provide an inviting pedestrian realm.

Source: City of Berkley



The mural shown above a Nova Chiropractic is one of 13 murals in Downtown Berkley.  
Source: City of Berkley DDA



The sketch up rendering above show how a buffer with trees, green space and trees would fit along side Berkley neighborhoods.  
Source: CWA



The building on Coolidge and Edgewood has upper story residential.  
Source: CWA

width, street trees and street furniture (light poles, benches, etc.) between the sidewalk and the roadway, and a defined area for semi-public activity, like outdoor dining or sidewalk sales. These elements combined are referred to as the “streetscape”. The streetscape of corridors should be designed for pedestrians first, and motorists second. A common streetscape program should be designed and implemented for Coolidge and Twelve Mile, based on the existing streetscape and plans for the Downtown Development Authority (DDA) with individual streetscapes for Eleven Mile Road and Woodward Avenue.

### Promote Public Art

Public art incorporated into public spaces, such as the murals in Berkley’s Downtown, promotes commercial investment, creates attractive sites for community and private events, fosters a sense of community ownership, and enhances the overall quality of life for residents and visitors. Working in partnership with organizations like the DDA, arts groups, private property owners and businesses, the City should promote public art in corridors through building relationships, ease of permitting processes, in-kind labor or services (city staff time or free trash pick-up), and possible financial support. Public art at key intersections and major destinations contributes to Berkley’s identity, fosters community pride, and provides visual interest for residents and visitors.

### Create Better Buffers

Due to the nature of Berkley’s layout, corridors more often than not abut single-family properties. This can create stress between the quiet residential neighborhoods and the bustle of commercial corridors. In order to achieve a better relationship, the City requires a landscaped buffer with a masonry wall, at least 10 feet in depth. Through zoning ordinance amendments, the City should maintain and strengthen this requirement, with encouragement of decorative walls and green alleys. The buffer should protect the well-being and investment of residents to the greatest extent possible. In certain portions of corridors, the typical lot sometimes cannot accommodate both the required buffer and parking. In these cases, re-purposing of residential lots may be considered

### Diversify Housing Choices in Corridors

Corridors are prime properties for new housing development in Berkley, including townhomes, courtyard apartments, and upper story lofts. A diversity of housing types allows for a range of options for various populations, ages, and incomes. Residents also then have the opportunity to stay in Berkley, at various stages in life. The City should allow additional housing types that fit within the character of each corridor, from upper story lofts in the downtown to duplexes on Greenfield and Eleven Mile. Design guidelines for each corridor are included later in this chapter.



The Berkley Public Library is an example of an institutional use on a corridor in Berkley.  
Source: CWA

### Mix Uses in Corridors

The Master Plan envisions the corridors in Berkley to fully transition from areas dominated by a single type of land use to a mix of uses. Mix of uses could include commercial, office, service, or residential. That mix is determined by the character of each area – the buildings, existing and surrounding land uses, the size and speed of each street and the function that corridor serves for Berkley as a community.



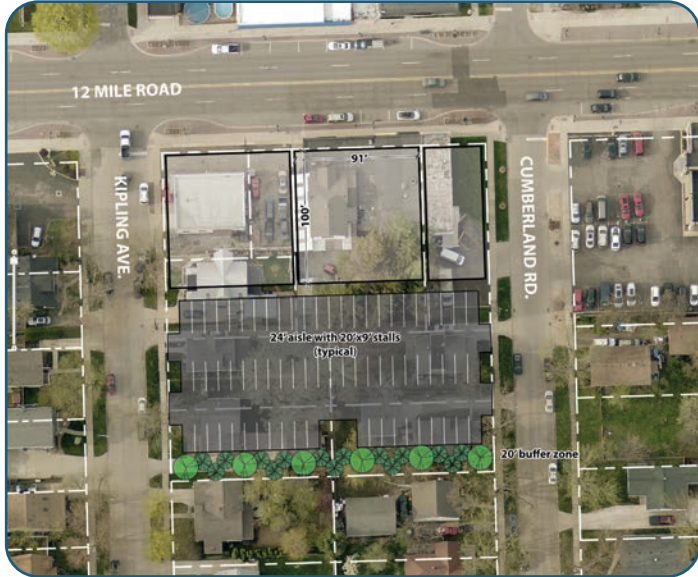
Restaurant uses, like Amici's, are mixed with other uses in Berkley's corridors.  
Source: CWA

The following sections in this chapter identify “character zones” and design guidelines along the



Coolidge features a mix of uses throughout the corridor.  
Source: City of Berkley

## Berkley Corridor Redevelopment Arithmetic



Source: Aerial Photograph - Oakland County. Graphis - CWA

City's corridors where mixed-use sites and corridors provide commercial, residential, and office space in a manner that builds on Berkley's best assets and protects neighborhoods. The character zones based on those in the 2019 Downtown Berkley Master Plan and the corridor future land categories of Residential Corridor, Downtown and Gateway Corridor.

While several factors limit redevelopment of property along Berkley's corridors, many of which cannot be addressed or fixed within a Master Plan, shallow depths, particularly lots 100 feet deep or less, are challenging for re-use or redevelopment:

- A standard lot is 40 feet wide by 100 feet deep
- 10 feet (buffer) + 40 feet (parking & access aisle) = 50 feet
- 40 foot width = 4 parking spaces maximum
- 2,000 square foot building –  
Retail = 7 parking spaces,  
Restaurants = 24 parking spaces

**Conclusion:**

There is not enough space to accommodate the buffer, parking and the building on the site.

## Recommendations

*Re-purpose residential property*

Commercial corridor properties can be expanded by re-purposing adjacent residential property. The "Potential Corridor Expansion/Transitional Buffer" future land use category indicates where residential property could transition to provide additional space for corridor properties. Such expansions should be considered on a case-by-case basis as part of a zoning approval process. The main part of that process is to ensure future commercial expansion does not adversely impact the adjacent residential parcels.

*Change parking regulations*

Relaxing parking requirements is appropriate when other parking options, such as municipal parking lots or shared parking, may not be possible.

## GREENFIELD

Greenfield is a five-lane high-speed road and the City's western boundary with Southfield. On Berkley's side, it is predominately single-family residential with a few multiple-family buildings and houses converted to office or retail uses. There are a variety of land uses on Southfield's side, from big-box commercial to multiple-family to a County park. The goal for the corridor is for the residential uses to be bolstered with adaptive re-use of houses for offices and retail, while also allowing for multiple-family development along Greenfield and possibly Ellwood.

Design guidelines for Greenfield and a concept plan for a multiple family development were created to showcase a possible development in the corridor.



The above photos are examples are appropriate to face Ellwood.



The above photos represent sample housing developments that are appropriate for Greenfield Road.

Source: Teska

## Greenfield Road Residential

The following planning principles, developed for the concept plan, apply to residential corridor areas on Greenfield and Ellwood:

- Buildings should front onto Ellwood Avenue and present a continuous front door appearance and lower building scale that is compatible with the neighborhood.
- Buildings located on Greenfield may be taller in scale, though not to exceed 3 stories in height.
- Buildings should be grouped into clusters to provide common green open spaces.
- No driveways should be on Ellwood Avenue. All driveway access should be on Greenfield Road or perpendicular side streets as possible.
- Resident parking should be internal to the development and landscaped.
- On-street visitor parking should be placed in small groupings and should be landscaped.
- Pedestrian walkways should provide access to common spaces and surrounding sidewalks.
- Sustainable site, building and landscaping elements are encouraged and should be incorporated as appropriate to the site and program.



The Greenfield Multiple Family Development concept plan, on page 59, is an example of the type of development that could be occur along the Greenfield corridor. Such development would be contingent upon the current owners wanting to sell their property, a developer wanting to design and build a multiple family complex, and the design conforming to all Zoning Ordinance requirements.

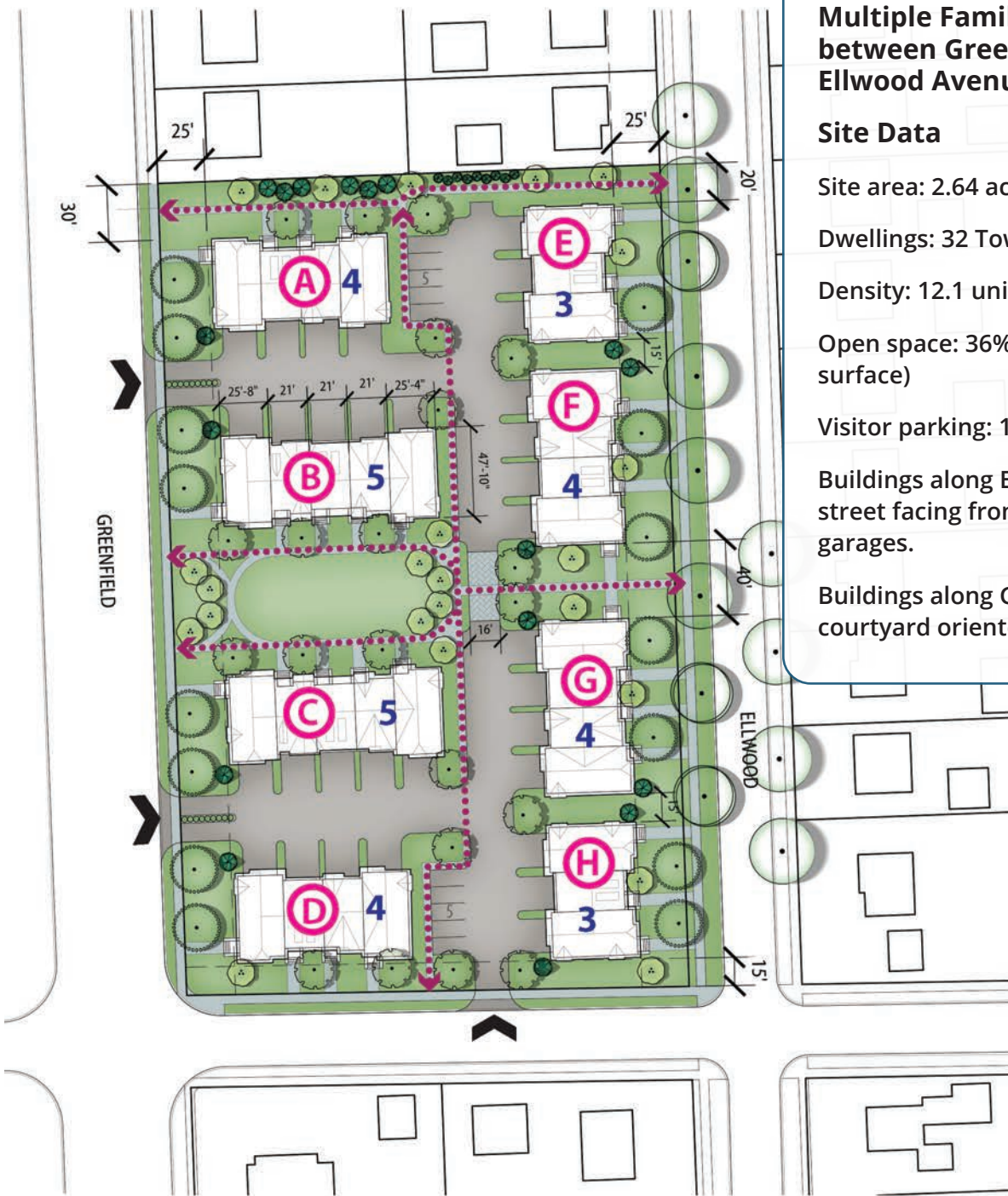
The conceptual plan includes the purchase of at least 16 parcels: eight (8) fronting Greenfield and eight (8) facing Ellwood. Such consolidation of parcels would only occur if all 16 property owners would want to sell their land. The City of Berkley will not be involved in purchases or taking of property and will not advise property owners in any capacity.

If a development such as the concept plan shown on page 59 were to be constructed, the following steps in the text box on this page would have to take place. The agreement of multiple individuals to sell their property is complicated and may prohibit the development of the size and scale of multiple family concept plan shown. A smaller scale development may be more practical but would still require the same cooperation of property owners, submission of site plans, and approval by the Berkley Planning Commission.

## Steps for Greenfield Road Concept Plan to be Built

- Agreement to sell properties from all current land owners to a single developer
- All properties combined into one parcel
- Consultation by the developer with Road Commission of Oakland County regarding the project
- Developer - working with architect, engineer, and other professionals - designs a site plan
- Proposed site plans must meet all zoning requirements, including but not limited to the following:
  - o Height
  - o Density
  - o Setbacks
  - o Parking Requirements
  - o Landscaping and screening
  - o Compatible design with neighborhoods
- Public notice to all property owners within 300 feet of the development of when and where Planning Commission will review the site plan. meeting date
- Approval by Road Commission of Oakland County for Greenfield access (may be condition of site plan approval)
- Site Plan must be reviewed and approved by the Berkley Planning Commission

Greenfield Road | Concept Plan



**Multiple Family Development between Greenfield Road and Ellwood Avenue**

**Site Data**

Site area: 2.64 acres

Dwellings: 32 Townhouses

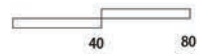
Density: 12.1 units per acre

Open space: 36% (permeable surface)

Visitor parking: 10 spaces

Buildings along Ellwood Ave have street facing front doors and rear garages.

Buildings along Greenfield Rd are courtyard oriented.





## ELEVEN MILE

Eleven Mile Road is a four-lane road along Berkley's southern border with Oak Park and Huntington Woods. The right-of-way is 66 feet with sidewalks and planting strips on both sides, varying in width from 6 to 12 feet. Due to the land uses and traffic patterns along Eleven Mile, the corridor is planned for three different character areas and corresponding future land use categories.

As redevelopment occurs on Eleven Mile, the City should implement unifying streetscape elements such as landscaping, seating, pedestrian lighting, and others as appropriate. Streetscapes should be coordinated with Oak Park and Huntington Woods, when possible

## Eleven Mile Design Guidelines

The following design guidelines apply to the entire corridor:

- Sidewalks should meet accessibility standards and should be appropriately lighted for bicycle and pedestrian traffic.
- Pedestrian crosswalks must meet accessibility standards and should be well marked.
- Areas adjacent to the curb should be planted with lawn and canopy trees, as space allows.
- On-street parking on side streets should be well defined. On-street parking on Eleven Mile is not allowed.
- Sustainable streetscape features such as permeable pavements, rain gardens and LED lighting are encouraged.



Eleven Mile  
Source: CWA

## Design Guidelines

The following design guidelines apply to Eleven Mile between Woodward and Mortenson:

- Buildings should front onto Eleven Mile. Front yard setbacks along Eleven Mile should match those of the adjacent existing homes.
- Buildings should be no taller than two stories.
- Attached single-family housing may range in groupings from 2 to 5 units per building.
- All driveway access should be on Eleven Mile Road or side streets, as possible.
- Pedestrian walkways should provide access from the front door to the sidewalk and parking areas, as well as common open spaces.
- Facades should incorporate front porches and clear visibility to front doors.
- Facades should incorporate a consistent rhythm of windows. Windows between lower and upper floors on adjacent buildings should relate to one another.
- Façade materials should be brick, stone, and wood to be consistent with existing housing types in Berkley.
- Rooflines should reflect the rooflines of this portion of Eleven Mile, which include gable and hip roofs.
- Rooflines should be arranged to provide visual interest and differentiation between units.
- Sustainable site, building and landscaping elements are encouraged and should be incorporated as appropriate.

### Woodward to Mortenson Residential Corridor

This section of Eleven Mile should remain primarily single-family housing. However, at intersections corners, attached single-family housing, such as duplexes and townhouses, could be allowed.



Single Family Houses on Eleven Mile  
Source: CWA



Office use in house on Eleven Mile  
Source: CWA



Duplex on Eleven Mile  
Source: CWA

## Mortenson to Robina Gateway Corridor

The intersection of Coolidge Highway and Eleven Mile is one of the most visible entrances to the City. While maintaining small-scale commercial and office uses, attached single-family housing should be allowed to enter the mix along this section of Eleven Mile. Redevelopment of older and obsolete buildings should include an architecturally pleasing façade treatment for the elevation that fronts Eleven Mile, much like the “before” and “after” photographs on this page. Public art, such as murals, should be encouraged.



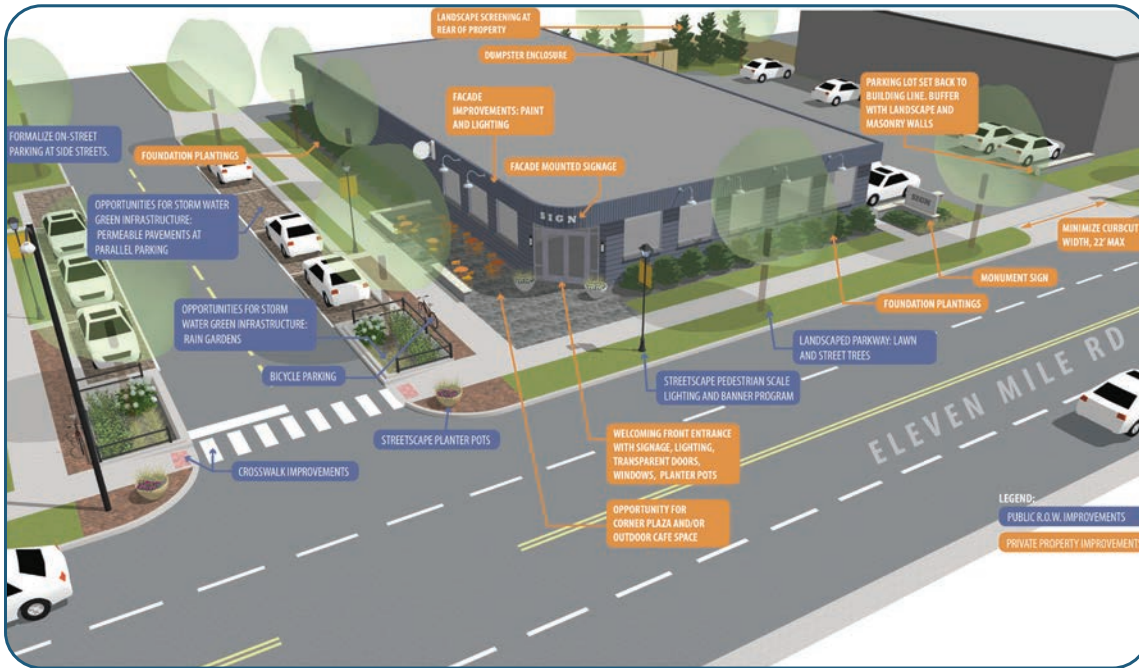
Photo examples show how modest building improvements, such as window replacements, paint, signage and lighting, have dramatically improved Berkley's local building character and appearances. Source: Google & Teska

## Design Guidelines

The following design guidelines apply to Eleven Mile, between Mortenson and Robina:

- Main building entrances should be prominent and visible from the street.
- Building windows should front onto the street.
- Driveways and parking areas should be clearly visible from the street and landscaped.
- Off-street parking lots, including those that abut the sidewalk, are screened with a masonry or decorative wall and landscaping.
- Monument style signs and building mounted signs should be incorporated into the site and building.
- Dumpsters should be screened from view via enclosures.
- Outdoor seating areas are encouraged as appropriate to building use.
- Building mounted lighting should complement the building.
- Sustainable features such as permeable pavements, LED lighting, native landscaping, locally sourced and recycled materials are encouraged.

## Eleven Mile Road | Concept Rendering & Design Guidelines



Source: Teska Associates

### Private building and parking enhancements:

- Main building entrances should be prominent and visible from the street.
- Building windows should front onto the street.
- Driveways and parking areas should be clearly visible from the street and landscaped.
- Monument style signs and building mounted signs should be incorporated into the site and building.
- Dumpsters should be screened from view via enclosures.
- Buildings and parking areas should be enhanced with landscaping.
- Outdoor seating areas are encouraged as appropriate to building use.
- Building mounted lighting should complement the building.
- Sustainable features such as permeable pavements, LED lighting, native landscaping, locally sourced and recycled materials are encouraged.

### Public right of way (ROW) enhancements:

- Sidewalks should meet accessibility standards and should be appropriately lighted.
- Pedestrian crosswalks must meet accessibility standards and should be well marked.
- Parkway should be planted with lawn and canopy trees as space allows.
- On-street parking should be well defined.
- Sustainable streetscape features such as permeable pavements, rain gardens and LED lighting are encouraged.

### Robina to Greenfield – Industrial/Retail

A unique mix of industrial and retail, this portion of Eleven Mile is starting to redevelop as a walkable corridor. The City should allow re-use of industrial buildings as lofts and other multiple-family uses here. The design guidelines as illustrated on this page should be implemented via zoning changes. Improvements within the public right-of-way (R.O.W.) and private properties should be coordinated to improve the overall corridor.

### WOODWARD

Traveling 21 miles through 11 communities, Woodward is one of the most studied and planned corridors in southeast Michigan. Regional plans for Woodward envision a multi-modal street with dedicated lanes for buses or trains, bicycles as well as vehicles with wide sidewalks and more intense development at transit stations. The 2020 Parks and Recreation Plan specifies connecting Berkley's trails and pathways to the regional system developing along Woodward Avenue in Oakland County.

In its 1.8-mile stretch in Berkley, Woodward is a divided boulevard with eight lanes of traffic and a 200-foot wide right-of-way. Roseland Park Cemetery occupies over a of a third of Berkley's frontage on Woodward and is a planned as Parks & Cemetery. The rest of the corridor in Berkley is planned as Gateway Corridor.

## Study Recommendations

The City is committed, to the greatest extent possible, to the following recommendations for Woodward in the 2012 Woodward Avenue TOD Corridor Study for South Oakland County and the 2015 Woodward Avenue Action Association Woodward Avenue Complete Street Plan.

- Reduction to six lanes of traffic, with expanded sidewalks, two-way raised cycle tracks, dedicated bus rapid transit.
- Bus rapid transit stops were planned at Eleven Mile and Twelve Mile, with increased development. Taller buildings at Twelve Mile and Woodward may not be possible due to the shallow lots in this area and deed restrictions of the original plat.
- Coordinated streetscape with adjacent communities include permeable paving, street lighting and landscaping.
- Parking policies requiring less parking where transit stops are within a 5-minute walk.



The Woodward Dream Cruise in an annual event on Woodward  
Source: City of Berkley

## Design Guidelines

The following design guidelines apply to East Twelve Mile:

- Connect sidewalks and trails along East Twelve Mile to the regional network along Woodward and the planned trail on the edge of Roseland Park Cemetery.
- Continue the street trees at regular intervals, pedestrian scale decorative lighting and wayfinding signs in other parts of Twelve Mile throughout this section.
- Repair and maintain the sidewalk on the south side of East Twelve Mile.
- Parking lot areas should be in the side yard, setback from the sidewalk and screened with a masonry or decorative wall and a landscaped area.
- New buildings should be no higher than two stories.
- Parking, loading, trash and service areas should be screened from the street and buffered from the adjacent neighborhood. In some cases, these may be in a side yard with appropriate screening to minimize neighborhood impacts.

## EAST TWELVE MILE

Between Woodward and Coolidge, this portion of Twelve Mile features Roseland Park Cemetery along its entire northern frontage. On the south side of the street, uses vary from medical offices with large parking lots to single family homes to vacant land. The lots are shallow, 115 to 135 feet in depth, and back up to the rear yards of the Oakland Manor neighborhood. Due to the lot sizes and surrounding land uses, this area is suited for single-family attached housing or small scale office, personal service or retail uses.



View of Roseland Cemetery from the south side of East Twelve Mile  
Source: CWA

## WEST TWELVE MILE

The portion of Twelve Mile west of the Downtown, from Greenfield Road to Buckingham Avenue, is planned as a Gateway Corridor. This portion of Twelve Mile has a second story residential and live/work units.

The 2018 Downtown Design Guidelines and the 2019 Downtown Master Plan have more in-depth recommendations that should be followed in addition to the design guidelines on this page.



Black Ritual Tattoo on West Twelve Mile.  
Source: CWA

## Design Guidelines

The following design guidelines apply to West Twelve Mile:

- Continue the brick pavers, street trees at regular intervals, pedestrian scale decorative lighting and wayfinding signs throughout this section of Twelve Mile.
- New buildings should be built to the street edge and have public and semi-public space.
- Outdoor product displays should be allowed and engaging storefront encouraged, with vibrant window displays.
- New buildings should be two stories at the street edge, with a third story allowed if stepped back from the street edge
- Buildings should transition and connect Twelve Mile to adjacent neighborhoods by stepping down in height and scale at rear property lines, buffering parking and landscaping at borders.
- The variety of building materials should be preserved and encouraged.
- Off-street parking lots, including those that abut the sidewalk, are screened with a masonry or decorative wall and landscaping.

## Design Guidelines

The following design guidelines apply to South Coolidge:

- Upper floor residential and townhouses are encouraged in a greater proportion than in the Downtown.
- Buildings have entrances directly to the street for pedestrians.
- Buildings may be at the sidewalk's edge or slightly setback to allow for landscaped yards, outdoor dining and display areas.
- Underutilized parking lots can be repurposed for dining patios, sitting areas or outdoor display areas.
- New development is two stories at the street edge, with an occasional third story set back from the street and adjacent neighborhoods.
- The use of traditional materials is promoted to maintain continuity within the Downtown.
- Underused parking lots are adapted to include beautification elements, access and passive use alternatives.
- Buildings and sites should transition to adjacent neighborhoods by stepping down in height and scale at rear property lines, buffering parking, and landscaping at borders.
- Curb extensions should be installed at intersections to aid pedestrian safety and provide landscaped areas.
- Off-street parking lots, including those that abut the sidewalk, are screened with a masonry or decorative wall and landscaping.
- Parking areas are buffered from the sidewalk and adjacent residential uses.
- Traditional pedestrian-scale light fixtures should be installed throughout this part of Coolidge.

## SOUTH COOLIDGE

Bordered by Catalpa to the north and Eleven Mile to the south, this area is envisioned to be a mixed-use area with residential, offices, restaurants and shopping, less dense than downtown but with the same degree of walkability.



Office use on South Coolidge  
Source: CWA



Ice cream shop on South Coolidge  
Source: CWA



## DOWNTOWN

Berkley's traditional downtown is on Twelve Mile, between Buckingham and Coolidge, and on Coolidge from Twelve Mile to Catalpa. The Downtown is home to distinctive buildings from the 1920's, 1930's, 1950's and 1960's, including the iconic Berkley Theater. Characterized by older buildings set close to the sidewalk, shops and restaurants, this area was designated as the "Downtown Core" in the 2019 Downtown Berkley Master Plan.

The 2018 Downtown Design Guidelines provide specific design guidelines for three distinct areas within the Downtown.



Repurposed street for outdoor dining in 2020.  
Source: City of Berkley



Gateway Mural at Twelve Mile and Coolidge in Downtown  
Source: City of Berkley

## Design Guidelines

The following design guidelines apply to Downtown Berkley:

- Buildings align at the sidewalk edge, with some variety allowed, to create a consistent street wall.
- New development is two stories at the street edge with a third story that is set back from the sidewalk and adjacent areas of lower scale, such as neighborhoods.
- The use of traditional materials is promoted to maintain continuity within the Downtown.
- Underused parking lots are adapted to include beautification elements, access and passive use alternatives.
- Institutional uses remain in the Downtown, to keep foot traffic and vibrancy.
- The streetscape is consistent and unified in appearance with curbside brick paving along the sidewalks, landscaping in corner planting areas and curb extensions, and low-scale, traditional light fixtures.
- On-street parallel parking is allowed throughout the Downtown, including on Coolidge.
- Off-street parking lots, including those that abut the right-of-way, are screened with a masonry or decorative wall and landscaping.



Source: Carlisle Wortman Associates